



docklands



Docklands North Lotts

Amending Planning Scheme, July 2006

Docklands North Lotts Planning Scheme (Amending Planning Scheme No.1) 2006

Amending Planning Scheme

Incorporating Ministerial Modifications

Made by Dublin Docklands Development Authority on the 8th December 2005 & Approved by the Minister for the Environment, Heritage & Local Government on the 26th day of June 2006

Note:

This Amending Planning Scheme must be read in conjunction with the North Lotts Planning Scheme 2002.

Dublin Docklands Development Authority

Chairperson

Lar Bradshaw

Council

Betty Ashe

John Boylan

Séan Carey

Enda Connellan

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Frances A. Corr

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Paul Maloney - *Chief Executive*

David Higgins - *Director of Finance*

Gerry Kelly - *Director of Social Regeneration*

Loretta Lambkin - *Director of Marketing*

John Mc Laughlin - *Director of Architecture*

Neil Mulcahy - *Secretary/Director of Property*

Department of the Environment, Heritage and Local Government

Dublin Docklands Development Authority Act, 1997

The Minister for the Environment, Heritage and Local Government pursuant to subsection (5) of section 25 of the Dublin Docklands Development Authority Act, 1997 (No. 7 of 1997) (hereinafter called "section 25") hereby approves, subject to the following modifications, the planning scheme for the North Lotts Area (the area as described in the attached schedule) (hereinafter called "the scheme"), which scheme was submitted to him by the Dublin Docklands Development Authority (hereinafter called "the Authority") on 19 January 2006:-

Modifications

- 1. Delete the final sentence in the first paragraph in section 3.1.2 (Residential).*
- 2. Substitute the third sentence in section 3.1.3 (Retail/Commercial) with the following:
"The Authority will seek to ensure that approximately 25,000 sq.m of net retail floorspace (18,000 sq.m of comparison retailing and 7,000 sq.m of convenience retailing) is to be developed in the Point Village in order to achieve critical retail mass in the centre."*
- 3. Substitute the second paragraph in section 3.2.2 (Building height) with the following:
"Section 5.3.4 of the 2002 Planning Scheme is amended to provide for the maximum parapet height of the Point Village landmark tower to be 100m above street level in a point block integrated form. The final paragraph of Section 5.3.4 is deleted and reference should be made to the EIS accompanying this document. Plant will be permitted internal to any architectural features permitted above this height as detailed in Section 5.4.4 of the 2002 Planning Scheme. The Authority will encourage the provision of a public viewing area on the top floor. Upon receipt of a Section 25 application for the landmark building, the Authority will consult with the Irish Aviation Authority regarding its requirements in relation to aviation obstacle warning lights. The maximum height of the roof structure of the Point Depot upon redevelopment shall be 28m above ground level."*
- 4. Substitute the conservation principles in section 3.3.2 (Conservation of Architectural Heritage) with the following:*

"Principle 1. Use

The existing use should be maintained or another major public/civic use should be facilitated.

The conservation of the Point Depot has been secured since 1988 through the introduction of a new use as an entertainment venue for the building. This new use has become synonymous with the building. The Authority's principal objective is the continued beneficial use of the protected structure.

The architectural elements of the Point Depot building which underpin its status as a protected structure should contribute to the urban form of the Point Square and north quays.

The revised Point Depot facility, or relevant parts of it, should contribute to urban life and activity within its context of a new Point Square and adjoining areas.

Principle 2. Balance between Commercial Demands and the Needs of the Protected Structure

In addition to the commercial necessity of having a viable use, the inherent character of the existing structure requires balanced consideration.

Any redevelopment of the Point Depot will be derived from the present physical form of the protected structure. The creative challenge is to recognise the needs of the protected structure in terms of its historic fabric and continued sense of identity while satisfying commercial requirements.

Principle 3. Plan Form

The original plan form and physical envelope of the existing Point Depot should be legible following any intervention.

The Point Depot originally comprised two main components: the masonry arcaded block to the river front and the train shed to the rear. The train shed was of significance because of its scale, its tripartite form of nave and galleried aisles and its wrought iron trussed roof together with the rubble walls and arcaded north façade.

The revised Point Depot building should express the protected structure.

The revised building envelope of the Point Depot, including its roof and upper perimeter floor levels should be modulated and articulated in a manner that is compatible with both the urban form of new adjacent buildings and the protected structure.

Principle 4. Fabric and Salvage

Interventions should retain the greatest amount of fabric so far as is practicable. Where material is removed from its original location it should be reused on site, and only in default of this should it be re-used elsewhere.

With the exception of some roof trusses, roof covering, some small amounts of masonry and flooring removed in the 1980's, all of the primary fabric persists. It is recognised that any intervention will inevitably generate unwanted material.

Minimising the intervention or impact on the historic fabric will need to be demonstrated. This will require an adequate design response from those engaged in amending the physical envelope of the existing Point Depot building.

Principle 5. Presentation to the River

The existing Point Depot building should generate and form the basis of an enlarged composition which should have regard to the ordering devices of the original protected structure and provide an appropriate setting and presentation to the River Liffey.

The bulk and massing of the revised building envelope of the Point Depot complex should fit into and contribute to the urban form of the river frontage and the proposed seven or eight storey buildings on the adjacent quays to the west.

Principle 6. Presentation to the East and West

The east and west flank walls of the depot building should be retained in any new design to the extent possible. All new openings formed through these walls should be detailed in a manner reflecting that of original openings. The walls need not be seen in a single prospect.

Any new external building envelope should express, in so far as is practicable, either internally or externally, the existing east and west flank walls.

Principle 7. Presentation to the new Point Square

The relationship between new and existing fabric on the northern elevation should follow an ordering system using devices of separation, change of material and plan.

The extension of the northern elevation for a revised Point Depot complex should be modulated and articulated in order to create a sense of space and location on arrival in the new public square. It should also express this elevation of the protected structure as a contributing façade to the urban space.

Principle 8. Volumetric Constraints

New development should have regard to the volumetric constraints outlined in the accompanying Diagram No. 6, Point Depot Volumetric Constraints.

The enlarged venue will be in a new urban context of seven and eight storey high buildings. Volumetric constraints in the diagrams should be taken as a guide to the limits of the external envelope of a revised building complex.

Principle 9. Contribution of an Enlarged Venue to the Urban Setting

Increasing capacity should be used as an opportunity to enhance architectural and spatial quality.

An enlarged Point Depot complex should integrate externally with its new urban setting, and the existing fabric of the protected structure should be a contributing factor.

The building should promote a sense of arrival and location upon entry into the area. Part of this will be in the form of an open concourse running from Point Square around to the entrance of the Point Depot on its eastern façade.

The opportunity exists to associate the new Point Square as a public space with the quayside promenade by linking it through a new plaza/concourse at the eastern entrance to the expanded Point Depot. In addition to bringing the perimeter of the building further into the public domain, this should be accomplished in a manner that gives an improved aspect to the riverside elevation of the Point Depot."

5. Substitute section 3.4.1 (The Point Depot) with the following:

"Any section 25 application for the substantial redevelopment of the Point Depot as an upgraded concert venue will have to demonstrate the following:

- Satisfactory traffic management measures are in place as part of event management, including adequate on-site accommodation for taxis and the provision (as appropriate) of special shuttle bus services to and from the city centre/public transport nodes***
- Event safety management plans are in place which shall (inter alia) provide for the safety of large numbers of pedestrians arriving and leaving the venue, and for the avoidance of any potential conflict with access for emergency vehicles***
- Adequate public transport capacity having regard to the delivery of LUAS and new rail facilities in the North Lotts area***
- Phasing and construction of the public car park, and the likely availability of spaces during events at the Point Depot***
- Servicing/access arrangements should not conflict with the LUAS."***

Schedule

In this Schedule –

Unless otherwise specified, any reference to a line drawn along any bridge, river, road or street, shall be construed as a line drawn along the centre of such bridge, river, road or street as the case may be.

That part of the City of Dublin bounded by a line commencing at the point (hereafter in this description referred to as 'the first-mentioned point') where the southerly projection of Guild Street intersects the River Liffey, then continuing in an easterly direction along the River Liffey to the point where it intersects the East Link Bridge (also known as the Ringsend Toll Bridge), then continuing in a

northerly direction along the East Link Bridge and East Wall Road to the point where it is joined by Sheriff Street Upper, then continuing in a westerly direction along Sheriff Street Upper and Sheriff Street Lower to the point where it joins Guild Street, then continuing in a southerly direction along Guild Street and the southerly projection thereof to the first-mentioned point.

***GIVEN under the Official Seal of the Minister
for the Environment, Heritage and Local Government this
26th day of June 2006.***

***Dick Roche
Minister for the Environment, Heritage and Local
Government***

Docklands North Lotts Planning Scheme
(Amending Planning Scheme No.1) 2006
Amending Planning Scheme

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The Amending Planning Scheme must be read in conjunction with the North Lotts Planning Scheme, 2002.

NOTE:

This document incorporates the modifications made by the Minister and, where appropriate, the modifications have been highlighted, italicised and bold text. Where a conflict arises between the Minister's modifications and the original content and diagrams of the planning scheme, the modifications take precedence.

1.1 Background

The North Lotts Planning Scheme, 2002¹ was made by the Dublin Docklands Development Authority on 19th July 2001 and approved by the Minister for the Environment and Local Government on 5th June 2002. This Amendment of the Planning Scheme is being made in accordance with the scope of the legislation governing the making of Planning Schemes.



1.2 Purpose of the Amendment

The purpose of the Planning Scheme is to provide robust and detailed direction for the planning and development of the relevant part of the North Lotts. The specific purpose of the Planning Scheme Amendment is:

- To review the height of the landmark building
- To facilitate the future development and operation of the Point Depot as a cultural facility
- To consider the future accommodation of LUAS
- To provide for social infrastructure
- To improve the urban design guidelines for retail development

The Authority carried out focussed consultations with relevant agencies and parties, in accordance with the relevant statutory requirements, during the preparation of this Amendment. The accompanying EIS assesses the environmental effects of the Scheme, including those associated with the tall building and the full retail impacts of the district centre.

1.3 Content

This document outlines key amendments to the 2002 Planning Scheme. The amendments are site specific, relating to the area described below in paragraph 2.1, and must be read and interpreted in conjunction with the 2002 Planning Scheme and the policies and objectives of the Dublin Docklands Master Plan, 2003. The amendments will be outlined under the following broad headings:

- The nature and extent of proposed development, including the proposed distribution and location of land uses

¹ Referred to as 2002 Planning Scheme in this Amendment

- Proposals in relation to the overall design of the proposed development, including maximum heights and the external finishes of structures
- Proposals relating to development of amenities and conservation of architectural heritage
- Proposals relating to transportation, including the roads layout, the provision of parking spaces and traffic management

The definitions contained in the 2002 Planning Scheme apply to this Amendment.

1.4 Planning Policy

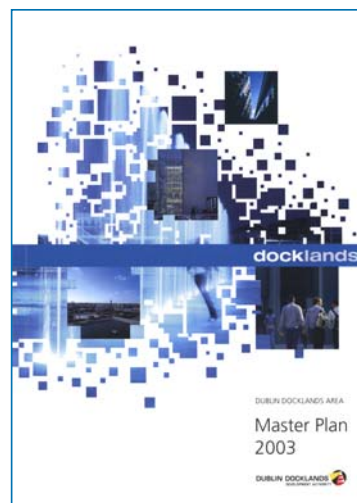
Relevant planning policy, additional to, or revised from that outlined in the 2002 Planning Scheme, is set out below.

1.4.1 Dublin Docklands Area Master Plan, 2003

The Master Plan outlines the broad strategic and planning context, policies and objectives for the Docklands Area. Important objectives for the area are included in the Master Plan.

The land use objective for the Campshires is Zone 9 -To preserve, provide and improve recreational amenity and open space. The zoning objective for the remainder of the land area is Zone 4 -To provide for and improve mixed services facilities. The water body is covered by land use zoning objective Zone 11 -To protect and improve canal, coastal and river amenities.

It is a policy of the Master Plan that the Point Village be developed as a district centre accommodating a significant level of retail floor space in association with complementary land uses such as cinemas, offices,



restaurants, public houses, recreational, educational, cultural and community uses. It is also a policy of the Master Plan that a 700-space public car park be located at the Point Village for short-term shopping, leisure and business use with a pricing structure to deter commuter use.

The Master Plan outlines the guiding principles of civic design under the headings of context, variety, permeability, legibility, movement, movement spaces, scale and height, architectural design and open spaces. Notably, the guidelines identify potential for permeability in new developments and the importance of proportionate building scale and height to spaces. In architectural terms, the importance of the design of corners and entrances is emphasised. The subject area is identified as an appropriate location for a high building. It is a policy of the Master Plan that the Authority will seek a coherent architectural expression to both sides of the river in order that the Liffey corridor can be read as an entity.²

It is stated that the Point Village provides an opportunity to provide hotel accommodation in conjunction with retail uses.

The Point Depot is highlighted as an existing cultural/tourist facility. It is also a policy of the Master Plan to seek the development of a major public square at Point Square.

1.4.2 Dublin City Development Plan 2005-2011

The Master Plan and the Dublin City Development Plan are required to be consistent in terms of policies and objectives. The City Development Plan reflects the broad objectives and policies contained in the Master Plan for this area.

1.4.3 Other relevant plans and policy

Regard will be had to other relevant plans and policy relating directly or indirectly to the subject Amendment including: A Platform for Change - Strategy 2000-2016, the Retail Planning Guidelines, the Residential Density Guidelines, the Social Housing Design Guidelines, the Childcare Facilities Guidelines, the Architectural Heritage Guidelines and the Dublin City Development Plan 2005-2011 (including the Part V Housing Strategy). The Greater Dublin Area Retail Planning Strategy confirms a district centre at the Point Village.



² Master Plan Policy 11, Paragraph 6.1.3

2.1 The Area

The subject area, as described in the 2002 Planning Scheme, incorporates the Point Village area which includes the Point Depot venue. It is bounded by Sheriff Street to the north, East Wall Road to the east and the River Liffey to the south. It is equivalent to Zone 7 as described in the 2002 Planning Scheme and is indicated in Diagram 1.



The main existing land use on the site is the Point Depot venue which is used for cultural and musical events. A rail line crosses the site but will not be required by Iarnród Éireann into the future.

2.2 Urban Analysis

The area occupies a landmark gateway location at the end of the northern quays. The area also marks the easternmost part of the evolving docklands as it meets Dublin Port and is at a nodal point for intense vehicular traffic to and from the East Link Bridge, the north quays and the East Wall Road. The development of Point Square and the completion of the Campshires amenity scheme will both serve to ensure an increased level of pedestrian activity to the area, and an appropriate mix and level of different land uses needs to reflect the anticipated intensification within the area.

In visual terms, the Point Depot is already a familiar landmark which is visible from all along City Quay, Sir John Rogerson's Quay and from the East Link approach road. The prominence of the Point Depot itself, however, depends to a large extent on the level of activity, container storage and the presence of ships in the port. The only other permanent and prominent structures at present on the skyline are St. Laurence O'Toole's Church and the Dublin Port offices.

The prominent location of the site is deserving of and will support development which will be high profile in nature. While the Point Depot itself is seen as a local landmark, the potential for elevating the profile of the area to having a citywide landmark status needs to be harnessed. This potential, already recognised in the 2002 Planning Scheme, is reassessed in the context of the current amendments to that



Scheme, and in the context of other high buildings planned in the Docklands and the City as a significant and visual point of reference.



2.3 Development Context

The area remains predominantly undeveloped at present with the Point Depot dominating the area. The Campshires improvement and development to the south is nearing completion and will represent an important amenity attraction in the area. It is envisaged that the East Wall Road will be redeveloped to accommodate the increased traffic from the Dublin Port Tunnel, requiring a land take of approximately 7.5 metres on the eastern edge of the area.

A services tunnel under the Liffey and rising main to the Ringsend sewage treatment plant, which will serve the North Lotts area, is scheduled for completion in 2007. Phasing of development will have to have regard to the delivery of this essential infrastructure. Interim measures to service development completed in advance of this infrastructure (e.g. on-site treatment) will be considered on their merits. Watermains infrastructure will also have to be upgraded to serve development. The ESB require the provision of a 38kv underground power line along East Wall Road.

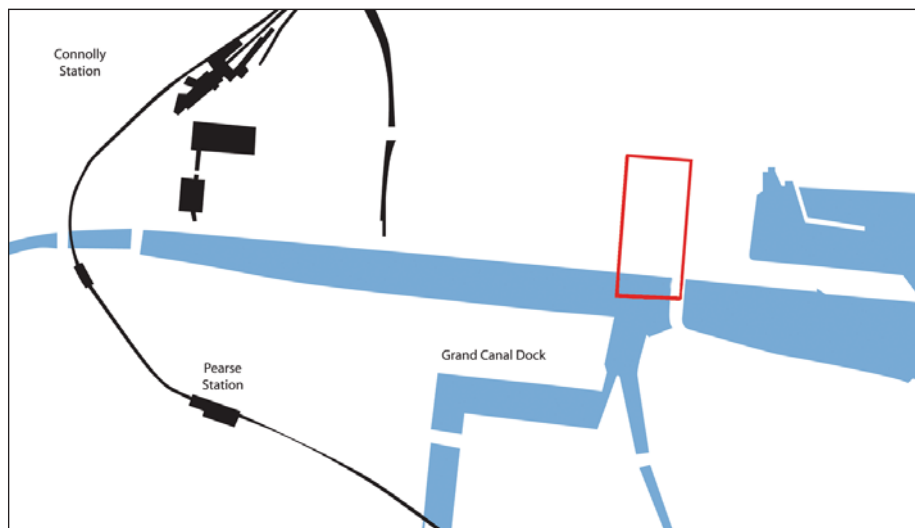


Diagram 1. Area Context

Unless explicitly so stated, the existing policies and provisions of the 2002 Planning Scheme, remain in effect. Additional policies and provisions have been included reflecting other updated policies, which post-date the 2002 Planning Scheme.



3.1 Nature and Extent of Development including Distribution and Location of Land Uses

3.1.1 Introduction

Diagram 2 indicates the distribution of land uses for the subject area. The provisions of this Section and Diagram 2 supersedes and/or amends Policy 4.12 (2) of the 2002 Planning Scheme.

3.1.2 Residential

Residential development will be focussed in the western portion of the area. All proposals for residential development will have to have regard to other noise sources, including the Port and the Point Depot. Any application for noise sensitive residences and premises affected by existing and future noise emitting uses shall have regard to the provisions of 3.1.7.

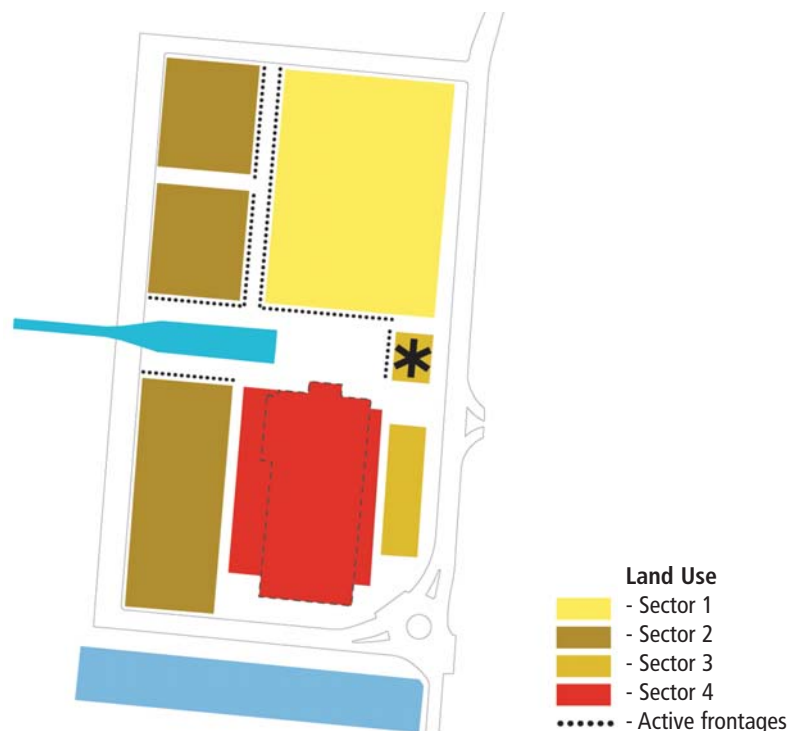


Diagram 2. Land Use

The Planning Scheme requires, in respect of residential development, at least 8 sq.m per bed space of private or semi-private open space. The Authority will, however, in assessment of Section 25 applications, where it relates to a residential use within the Point Village landmark tower, be prepared to reduce this requirement if it can be demonstrated by the applicant that there is sufficient public open space.

Paragraph 15.9.2 and 15.9.3 of the Dublin City Development Plan 2005-2011 relating to size and layout of units shall apply to new development, in addition to the general provisions of the Social Housing Guidelines, or any other applicable Ministerial guidelines that may supercede these guidelines.

3.1.3 Retail/Commercial

The Point Village has been identified as a suitable location for the development of a district centre scale retail scheme with related cinemas, restaurants, etc to serve the Docklands. The north eastern part of the subject area, bounded by Sheriff Street and East Wall Road, should be developed with a particular concentration of these uses.

“The Authority will seek to ensure that approximately 25,000 sq.m of net retail floorspace (18,000 sq.m of comparison retailing and 7,000 sq.m of convenience retailing) is to be developed in the Point Village in order to achieve critical retail mass in the centre.”

3.1.4 Cultural/Social Infrastructure

The Point Depot is identified as an important cultural use with city-wide significance. The Authority will seek to support the development of the Point Depot to ensure its future viability as a venue and to provide a world class venue capable of attracting significant performances. The protected status of the Point Depot will need to be addressed in any future design proposals. A full noise assessment shall be submitted as part of any Section 25 application for the redevelopment of the Point Depot as a music venue.

Public leisure and community uses are envisaged to be provided in the Point Village in association with other commercial uses. The Authority will seek the provision of a public leisure centre/swimming pool, crèche, medical centre and library in the area. A place of public worship will be encouraged.

3.1.5 Public Car Park

The 700-space public car park should be located underground or alternatively any multi-storey car park above ground should not front onto the Square, the quays or the north/south street along the western boundary of the area.

3.1.6 Land Use Categories

The land use categories detailed below shall apply to the area. Those uses falling in the 'permitted' category will be those which should be the principal uses for the relevant sector. Those falling within the 'open for consideration' category will only be permitted where they do not undermine the provision of uses promoted in the permitted category.

Unless otherwise stated, social infrastructure as detailed in 3.1.4 above shall be classed as residential for the purposes of the land use categories in this Planning Scheme Amendment. Retail use shall include bars, restaurants and other public counter uses associated with a district centre.

Zone 1	<u>Permitted</u> Retail, leisure, public car park <u>Open for Consideration</u> Residential, office
Zone 2	<u>Permitted</u> Mixed use in accordance with Policy 4.12 (2) of the 2002 Planning Scheme, with a predominance of ground floor retailing. <u>Open for Consideration</u> Reduction in residential where provision of residences would result in disamenity for future residents. Public car park
Zone 3	<u>Permitted</u> Hotel, leisure, cultural, offices, social infrastructure, active ground floor uses <u>Open for Consideration</u> Residential
Zone 4	<u>Permitted</u> Theatre, major public/civic use

3.1.7 Noise

The following design goals shall apply to noise sensitive premises affected by music noise and existing port activities (with or without the redevelopment of the Point Depot as a music venue) and to breakout music noise from the redeveloped Point Depot.

Design Goals

Criterion	Noise Limit – dB(A) _{Leq 15mins}
Internal noise limit for noise sensitive premises	20-25 dB(A)
Point Depot entertainment breakout noise limit	56 dB(A)

Any Section 25 application shall be accompanied by sound insulation details, taking into account tonal impulse emissions, for approval by the Authority.

3.2 Overall Design

3.2.1 Urban and Block Structure

The predominantly orthogonal grid form of the area is retained to reflect the footprint of existing structures to be retained and the current land holdings as much as is practicable. Diagram 3 illustrates the block structure for the subject area. The block structure will permit the appropriate extent and mix of land uses within the area while enhancing the city-wide legibility of the landmark character of the Point Village.

The required increase in capacity of the Point Depot is such that the building will require significantly increased pedestrian access and egress facilities. While there are opportunities for making such provisions on the north, south and east of the building, the preferred route for the larger portion of pedestrians is via the eastern façade to the North Wall Quay so as to facilitate large groups in short spaces of time and to maximise personal safety in crowded situations. Access/egress for patrons will be dispersed on the northern, eastern and southern sides of the Point. This will also control the spread of crowds onto the LUAS station and the quays.

The location of Point Square is a natural termination of the Mayor Street axis and offers a sense of arrival by virtue of its scale and the mix of uses around its perimeter. It also provides for the possible further extension of LUAS.

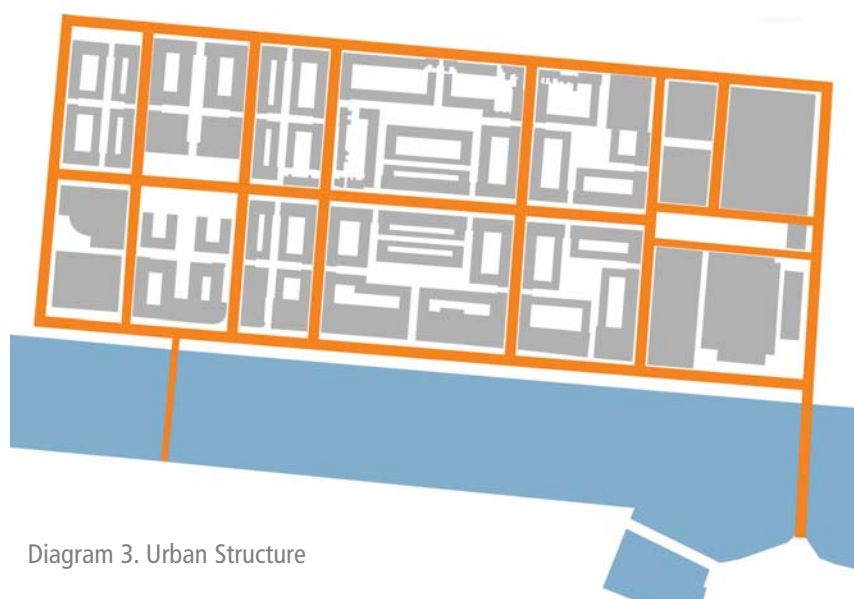


Diagram 3. Urban Structure

The landmark tall building can assert itself locally by being on the Mayor Street axis and at a prominent location within the Point Square, and will have a wider City presence by virtue of its height, its exceptional design quality and its relationship to the River.

The north-western and north-eastern parts of the area will have a strong retail presence. A north/south pedestrian priority area should therefore be provided between these two retail areas as shown (indicatively) in Diagram 7. By virtue of its relative distance to the noise-producing elements such as the Point Depot, the East Wall Road and the Dublin Port, the north-western area is also particularly suited to the provision of residential accommodation at the upper levels.

The North Wall Quay frontage will be expressed primarily by the Point Depot in its extended form. The design of the extension, in terms of scale, massing, detail and its composition with the other elements of the area, will be of particular importance in enhancing the landmark character of the area and having due regard to the protected structure status of the existing building.

The scale of development to the north of the area will reflect the large-scale retail use. The East Wall Road and Sheriff Street frontages can be bold and large in scale, but any new streets or spaces within the site serving predominantly residential areas, whether they are at or above ground level, will be modified to reflect an intimate human scale.

All frontages to Point Square should be enlivened with day and night time activities such as circulation, cafés, restaurants, etc., so as to create an animated space and enhance the character of this landmark location. Shop frontage from the retail block directly onto Point Square will not be permitted where that frontage is likely to be secondary in nature. Frontage for fastfood takeaway outlets will be restricted to avoid domination of Point Square.



The new eastern elevation to the Point Depot and the southern elevation of the landmark tower structure will be of such a scale and composition as to underpin the importance of this area as an important landmark and gateway to the City.

New development shall have regard to the provisions and recommendations of the Greater Dublin Strategic Drainage Study in relation to flood protection measures.

3.2.2 Building height

Maximum numbers of storeys are prescribed for both commercial and residential development as shown in Diagram 5. The Authority will apply the principles of the 2002 Planning Scheme and of Diagram 5 in particular to determine the appropriate height for mixed use buildings.

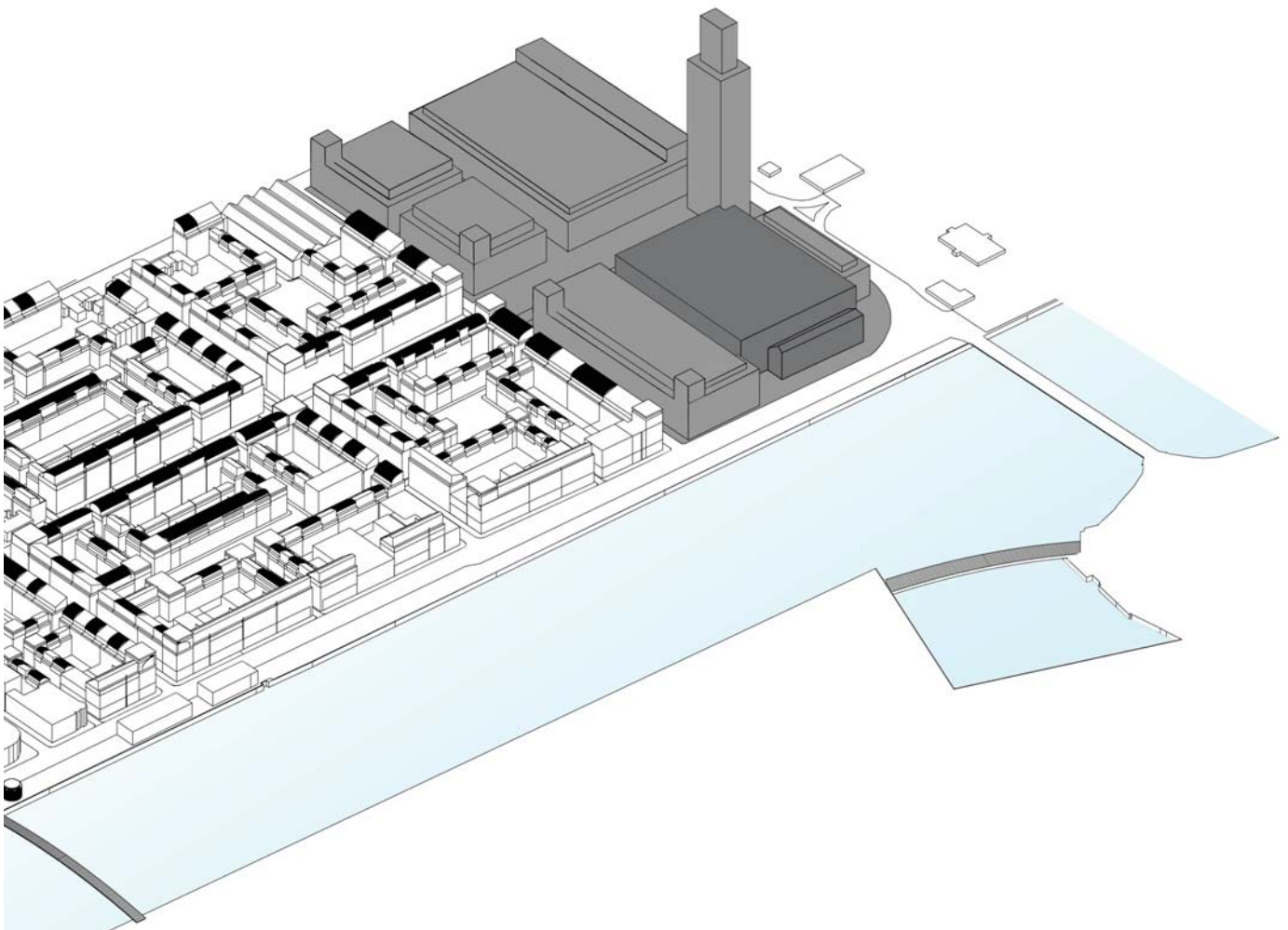


Diagram 4. Indicative View

"Section 5.3.4 of the 2002 Planning Scheme is amended to provide for the maximum parapet height of the Point Village landmark tower to be 100m above street level in a point block integrated form. The final paragraph of Section 5.3.4 is deleted and reference should be made to the EIS accompanying this document. Plant will be permitted internal to any architectural features permitted above this height as detailed in Section 5.4.4 of the 2002 Planning Scheme. The Authority will encourage the provision of a public viewing area on the top floor. Upon receipt of a Section 25 application for the landmark building, the Authority will consult with the Irish Aviation Authority regarding its requirements in relation to aviation obstacle warning lights. The maximum height of the roof structure of the Point Depot upon redevelopment shall be 28m above ground level."

3.2.3 External Finishes

The palette of materials and colours selected for the buildings should set this area apart from the evolving North Lotts to the west of the area. Materials for buildings should provide warmth and variety of colour, so as to give them their own distinctive character.



Diagram 5. Building Heights



3.3 Development of Amenities including Conservation of Architectural Heritage

3.3.1 Public space and amenities

All public space in the subject area is to be developed to the highest quality. The Authority may determine the surface landscape works for the principal public spaces. The Authority may elect to reserve the right to undertake these works by condition in any Section 25 certificate and to impose related charges also by condition in any certificate. The Point Village Square shall be a minimum of 55.6m up to a maximum of 65m in width from north to south. Where any width less than the maximum is being considered, the Authority will require the applicant to provide alternative open, public amenity space satisfactory to the Authority within their development.

3.3.2 Conservation of Architectural Heritage

In addition to having regard to the Architectural Heritage Guidelines, conservation principles detailed in this section have been devised specifically for the Point Depot. The following principles will be applied in the assessment of any Section 25 application relating to the development of the Point Depot.

“Principle 1. Use

The existing use should be maintained or another major public/civic use should be facilitated.

The conservation of the Point Depot has been secured since 1988 through the introduction of a new use as an entertainment venue for the building. This new use has become synonymous with the building. The Authority’s principal objective is the continued beneficial use of the protected structure.

The architectural elements of the Point Depot building which underpin its status as a protected structure should contribute to the urban form of the Point Square and north quays.

The revised Point Depot facility, or relevant parts of it, should contribute to urban life and activity within its context of a new Point Square and adjoining areas.

Principle 2. Balance between Commercial Demands and the Needs of the Protected Structure

In addition to the commercial necessity of having a viable use, the inherent character of the existing structure requires balanced consideration.

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The original plan form and physical envelope of the existing Point Depot should be legible following any intervention.

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Interventions should retain the greatest amount of fabric so far as is practicable. Where material is removed from its original location it should be reused on site, and only in default of this should it be re-used elsewhere.

With the exception of some roof trusses, roof covering, some small amounts of masonry and flooring removed in the 1980's, all of the primary fabric persists. It is recognised that any intervention will inevitably generate unwanted material.

Minimising the intervention or impact on the historic fabric will need to be demonstrated. This will require an adequate design response from those engaged in amending the physical envelope of the existing Point Depot building.

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The existing Point Depot building should generate and form the basis of an enlarged composition which should have regard to the ordering devices of the original protected structure and provide an appropriate setting and presentation to the River Liffey.

The bulk and massing of the revised building envelope of the Point Depot complex should fit into and contribute to the urban form of the river frontage and the proposed seven or eight storey buildings on the adjacent quays to the west.

Principle 6. Presentation to the East and West

The east and west flank walls of the depot building should be retained in any new design to the extent possible. All new openings formed through these walls should be detailed in a manner reflecting that of original openings. The walls need not be seen in a single prospect.

Any new external building envelope should express, in so far as is practicable, either internally or externally, the existing east and west flank walls.

Principle 7. Presentation to the new Point Square

The relationship between new and existing fabric on the northern elevation should follow an ordering system using devices of separation, change of material and plan.

The extension of the northern elevation for a revised Point Depot complex should be modulated and articulated in order to create a sense of space and location on arrival in the new public square. It should also express this elevation of the protected structure as a contributing façade to the urban space.

Principle 8. Volumetric Constraints

New development should have regard to the volumetric constraints outlined in the accompanying Diagram No. 6, Point Depot Volumetric Constraints.

The enlarged venue will be in a new urban context of seven and eight storey high buildings. Volumetric constraints in the diagrams should be taken as a guide to the limits of the external envelope of a revised building complex.

Principle 9. Contribution of an Enlarged Venue to the Urban Setting

Increasing capacity should be used as an opportunity to enhance architectural and spatial quality.

An enlarged Point Depot complex should integrate externally with its new urban setting, and the existing fabric of the protected structure should be a contributing factor.

The building should promote a sense of arrival and location upon entry into the area. Part of this will be in the form of an open concourse running from Point Square around to the entrance of the Point Depot on its eastern façade.

The opportunity exists to associate the new Point Square as a public space with the quayside promenade by linking it through a new plaza/concourse at the eastern entrance to the expanded Point Depot. In addition to bringing the perimeter of the building further into the public domain, this should be accomplished in a manner that gives an improved aspect to the riverside elevation of the Point Depot."

3.4 Transportation

The transportation and access arrangements for the site are illustrated in Diagram 7.

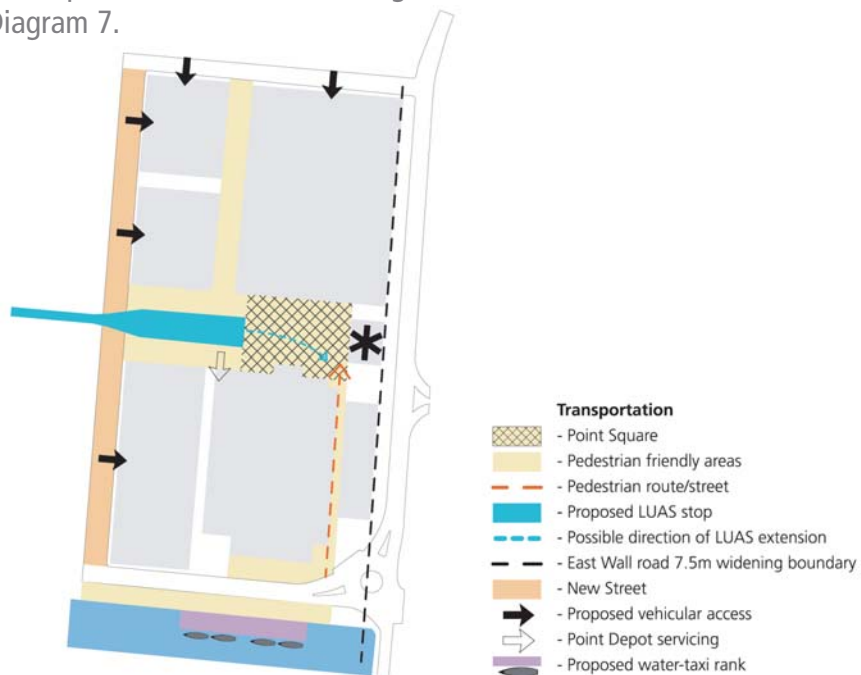


Diagram 7. Transportation

3.4.1 The Point Depot

“Any section 25 application for the substantial redevelopment of the Point Depot as an upgraded concert venue will have to demonstrate the following:

- ***Satisfactory traffic management measures are in place as part of event management, including adequate on-site accommodation for taxis and the provision (as appropriate) of special shuttle bus services to and from the city centre/public transport nodes***
- ***Event safety management plans are in place which shall (inter alia) provide for the safety of large numbers of pedestrians arriving and leaving the venue, and for the avoidance of any potential conflict with access for emergency vehicles***
- ***Adequate public transport capacity having regard to the delivery of LUAS and new rail facilities in the North Lotts area***
- ***Phasing and construction of the public car park, and the likely availability of spaces during events at the Point Depot***
- ***Servicing/access arrangements should not conflict with the LUAS”.***

3.4.2 LUAS

The Authority seeks to make provision to accommodate a potential extension of the LUAS line through the subject area. Following consultation it has emerged that the most likely route for an extension of the LUAS line is to continue the existing proposed line through the area to the south. This is indicated in Diagram 7 where provision has been made for the possible extension of the LUAS to the south.

3.4.3 Parking

Section 7.5.6 of the 2002 Planning Scheme is amended to require that the public car park will be subject to a pricing regime to deter long stay commuter parking. This car park shall serve the short-term parking requirements of the Point Village, catering for the needs of the Point Depot Venue, district retailing, bars and restaurants.



Dublin Docklands Development Authority
52-55 Custom House Quay, Docklands
Dublin 1, Ireland.

T +353 1 818 3300 F +353 1 818 3399
www.dublindocklands.ie

€5.00